

## Building Location and Massing

To achieve the urban design intentions of the Core Area plan, a series of specific site controls governing the placement and scale of buildings (the building envelope) in the Mixed-Use and Residential Character Districts has been defined. These are summarized in the Building Controls diagram which designates five types of controls:

‡ *Building Setback:* a line that defines the outside limits of where buildings can be located on a site. Exterior walls may be positioned any distance behind the setback line, but no part of the building can extend any closer to the edge of the property. Setbacks are designated where a minimum dimension of open area is required, for health and safety or aesthetic reasons.

‡ *Build-to Line:* a line that defines precisely where buildings are to be located on a site. Unlike a setback, build-to lines leave little flexibility in the placement of exterior walls. They are prescriptive, rather than preemptive, and are designated where continuity of building mass is important, for example, in maintaining a strong street edge, or defining the enclosure of a public space. In most cases, the required Build-to Line is coincident with existing property boundaries. The exceptions to this are: 1. Montezuma Road, south and north side from 55th Street to Campanile Drive. In this area the Build-to Line is set back 15 feet from the property boundary, and, 2. College Avenue, from Lindo Paseo to Montezuma. On the west side, the Build-to Line extends directly south from the corner point at Lindo Paseo and College as opposed to following the curvy curb. On the east side, the Build-to Line describes a larger radius arc to provide additional sidewalk area at the intersection.

‡ *Building Height:* the **maximum** number of stories that can be built within the zone designated. For purposes of calculating maximum building height (in linear feet), this Urban Design Plan defines a ground floor story as a maximum of 20 feet (to allow for lobbies, etc.) with subsequent stories at a maximum of 12 feet each.

‡ *Open Space:* the **minimum** area of open space that must be provided on a specific site. This can be established quite precisely for the superblocs of the Mixed-Use District, but in the Residential District, where property assembly objectives are unclear, it is defined as a percentage (40%) of total site area.

‡ *Pedestrian access:* critical pedestrian connections that must be provided through a development parcel. The lines shown on the Building Controls diagram indicate generally where pedestrian connections must be provided, not the specific location, width or configuration of the routes.

### Mixed-Use District

Building location and massing controls are relatively extensive in the Mixed-Use District to maintain a reasonable degree of clarity and order in the exciting commercial environment that is intended for this area. In keeping with the

superblock concept, however, the controls concentrate on establishing well defined streets and a strong image of evenly scaled, integrated development on the exterior, while the configuration and detailed character of interior spaces is appropriately left to the talents and imagination of future project designers.

### Mixed-Use District Build-to Lines:

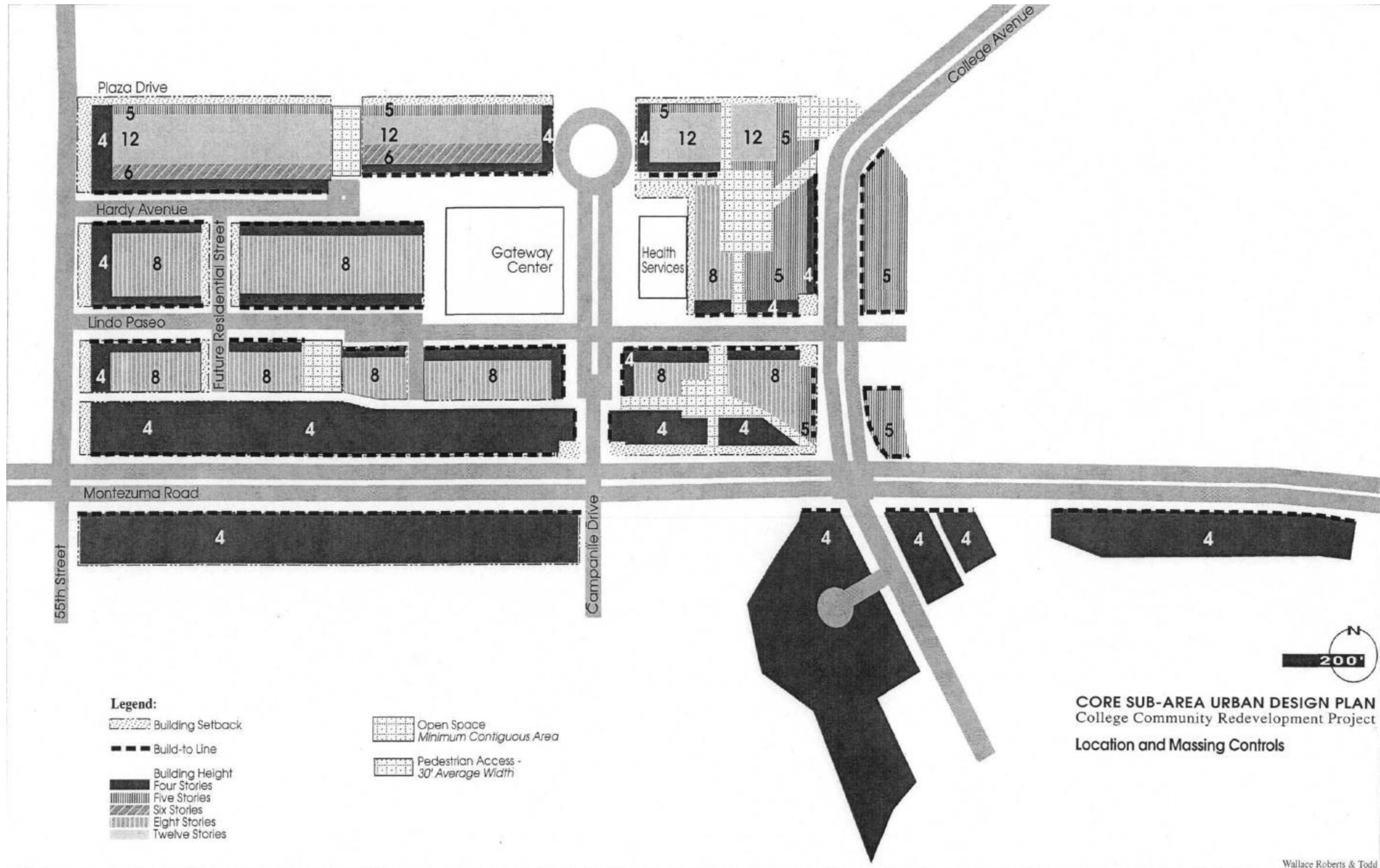
‡ To strengthen the movement corridors through the District, build-to lines are established along property boundaries on all street frontages. Exterior building walls must be located along at least 75 percent of these lines. Facade articulations, including recessed entries, should be restricted to maintain the continuity and clarity of the building line. Typical articulation limits would space them at least 30 feet apart on center, and restrict their size to no larger than 15 feet wide by 4 feet deep.

‡ At the intersection of College and Montezuma, the build-to line remains straight on College's west side and peels back on the east side to dramatize the skewed geometry of this focal intersection and to provide additional area at the corners for a special gateway expression, created by landscape, lighting, signage, a public art installation, or some appropriate combination of these approaches.

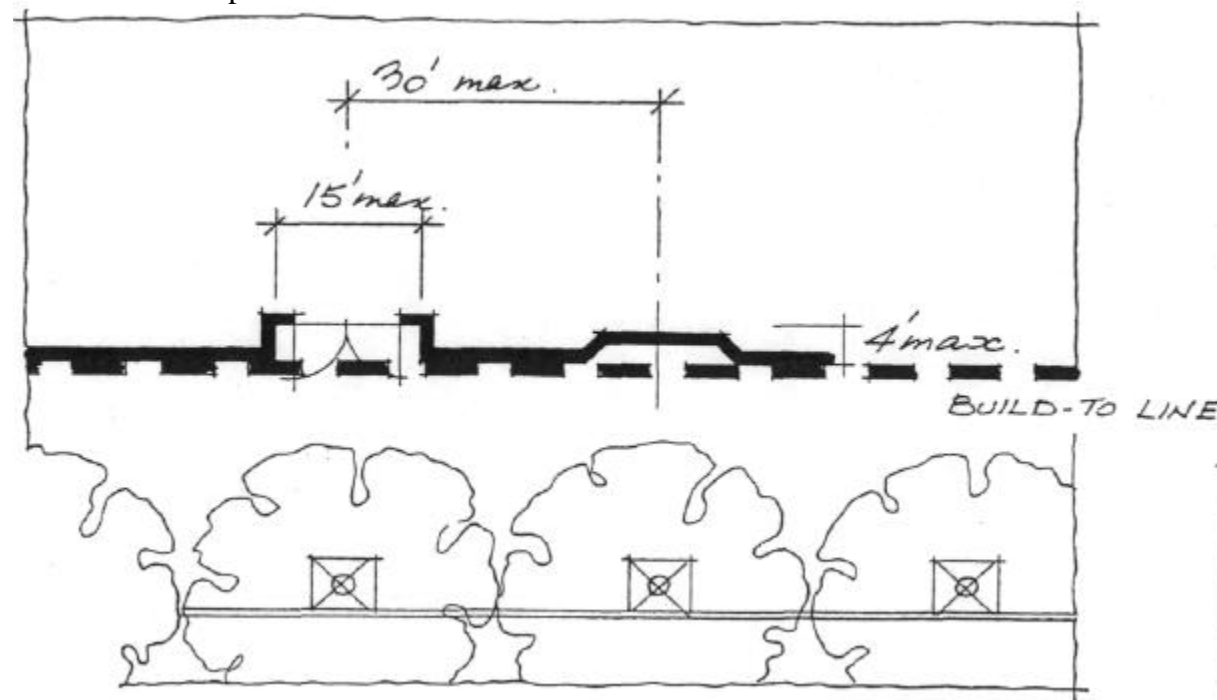
‡ At the interface with the Campus District, the build-to line along Campanile Drive encourages a gateway at the intersection of Montezuma and widens as it approaches Lindo Paseo to open into the campus.

• On the northeast block of College Avenue and the north side of Montezuma, the build-to lines apply only to partially underground parking structures. On the east side of College, from the pedestrian bridge to Lindo Paseo, the sidewalk is elevated 4 feet above grade over a split level parking structure that is built out to the property line. This elevated sidewalk is intended to separate pedestrians from traffic, enhancing the opportunity for an active street frontage that connects directly to the pedestrian bridge into the campus. Commercial frontage is set back at least 20 feet from the build-to line at the edge of the sidewalk to allow outdoor seating and patios. There are no build-to lines or required setbacks for the alley (east) side of this property.

• On Montezuma, between College and Campanile, there is a natural depression of approximately 8 feet in the center of the block which provides an excellent opportunity for a partially concealed entry to below-grade parking. As on College, the Urban Design Plan recommends constructing the sidewalk over a parking deck which is built out to the property line. Working with the natural grades of the site, this walkway will be approximately 8 feet above the street and garage entry at the center of the block, returning to grade at College and Campanile. Commercial frontage is set back at least 12 feet from the build-to line at the edge of the sidewalk.



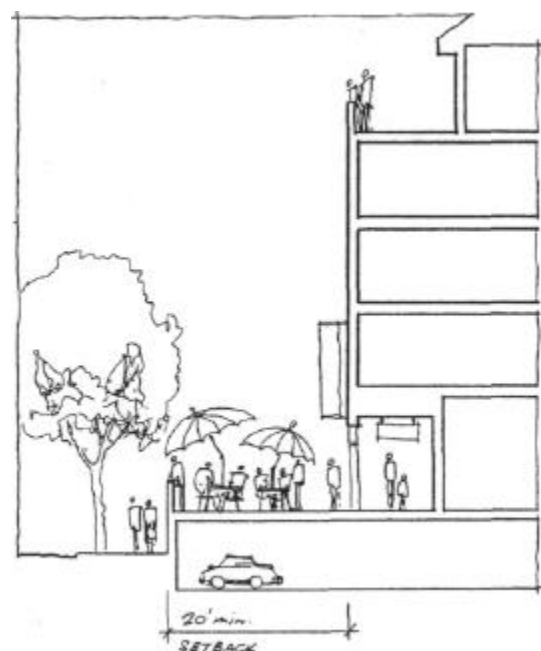
An additional build-to line is established on the interior of the northern block, along the north side of the former Hardy Avenue right-of-way. The purpose of maintaining a build-to line here is to establish a visual relationship with new residential development on the west side of Campanile, creating a sense of building continuity across the Transit Center and entry into campus.



*Build-to Line*

**Mixed-Use District Building Setbacks:**

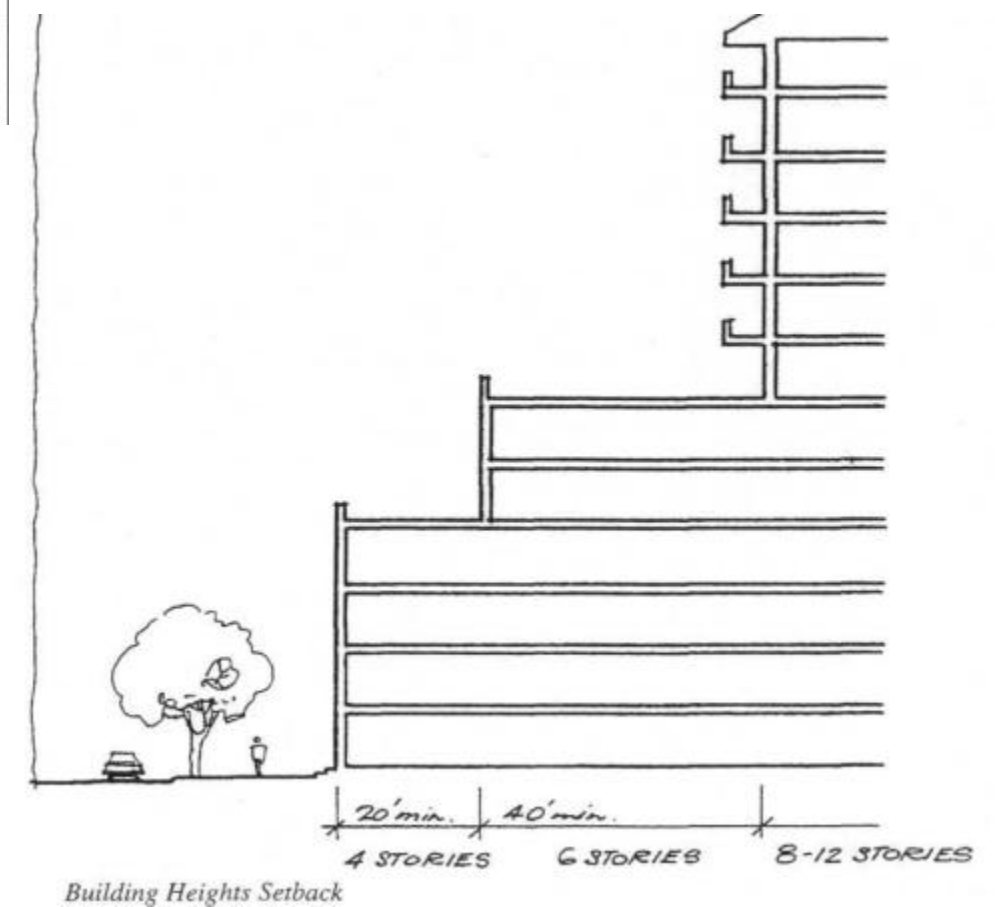
- | In addition to setbacks along the elevated sidewalks on College and Montezuma discussed above, the Plan requires a setback of 10 feet on Plaza Drive. This is to maintain a measure of flexibility at the boundary between the campus and the Core Area while the University studies the impacts of current and proposed development and its implications for the future role of this important pedestrian and service corridor.
- | Building Setbacks are also proposed to hold open key corners of the District where special gateway, signage or access elements are programmed. These include the northeast corner (College Avenue at Plaza Drive) where parking entry from College Avenue is proposed; the west sides of the College and Lindo Paseo intersection, and the Campanile gateway corner on Montezuma. While the intention to mark these locations with an appropriate expression of entry is a fixed element of the Urban Design Plan, the size and shape of the setbacks indicated are negotiable. For example, a more dramatic and visible expression that builds out to the property line instead of setting back at all is equally acceptable.



*College Avenue (east side) Setback*

**Mixed-Use District Building Height:**

- | Except on College Avenue, the maximum building height on all street frontages is restricted to four stories. To maintain continuity of scale across Campanile, this requirement also applies to the northern edge of the former Hardy Avenue right-of-way.
- | On College, the five story height limit specified in the Master Project Plan is maintained, recognizing the width of this major street and the objective of developing a high intensity commercial image in the College corridor. To soften the interface between the campus and Core Area development, five stories is also the maximum height permitted on Plaza Drive.
- | At a minimum upper level setback of 20 feet throughout, buildings may increase in height to the limits set in the Master Project Plan. Reinforcing the concept of stepping buildings down towards the neighborhood, the interior portions of the District may vary from 12 stories on Hardy Avenue, to 8 stories on the north side of Lindo Paseo, to 6 stories on the south side of Lindo Paseo.



### Mixed-Use District Open Space and Pedestrian Access:

- ‡ The Master Project Plan specifies a maximum site coverage for all development in the Core Area of 60 percent. It further stipulates that when retail and office uses exceed 50 percent of gross floor area in a mixed-use development, an additional 10 percent of the overall site area will be maintained as public open space. How qualifying open space areas are defined is not fully addressed.

The Core Sub-Area Urban Design Plan proposes that qualifying open space in the Mixed-Use District is defined as all open areas and public activity spaces (e.g. spaces other than building footprint) including open terraces and public balconies, arcades and covered pedestrian walkways, to a maximum of 18 feet above the primary activity level of the development (which may not be the natural grade of the site). It is also proposed that up to 25 percent of required open space may be provided off-site, so long as it is within the Mixed-Use Character District.

The Building Controls diagram shows a central open area in each of the development superblocks west of College Avenue, and various pedestrian access connections which qualify as part of the open space requirement. The areas shown, however, are only representative of the amount of space that must be provided and are not intended to indicate the specific location, configuration, or even number of required spaces and pedestrian routes. Beyond the minimum amount of open space and pedestrian connections that must be provided, decisions about their scale, shape, character and distribution are to be addressed as an integral part of detailed project design.

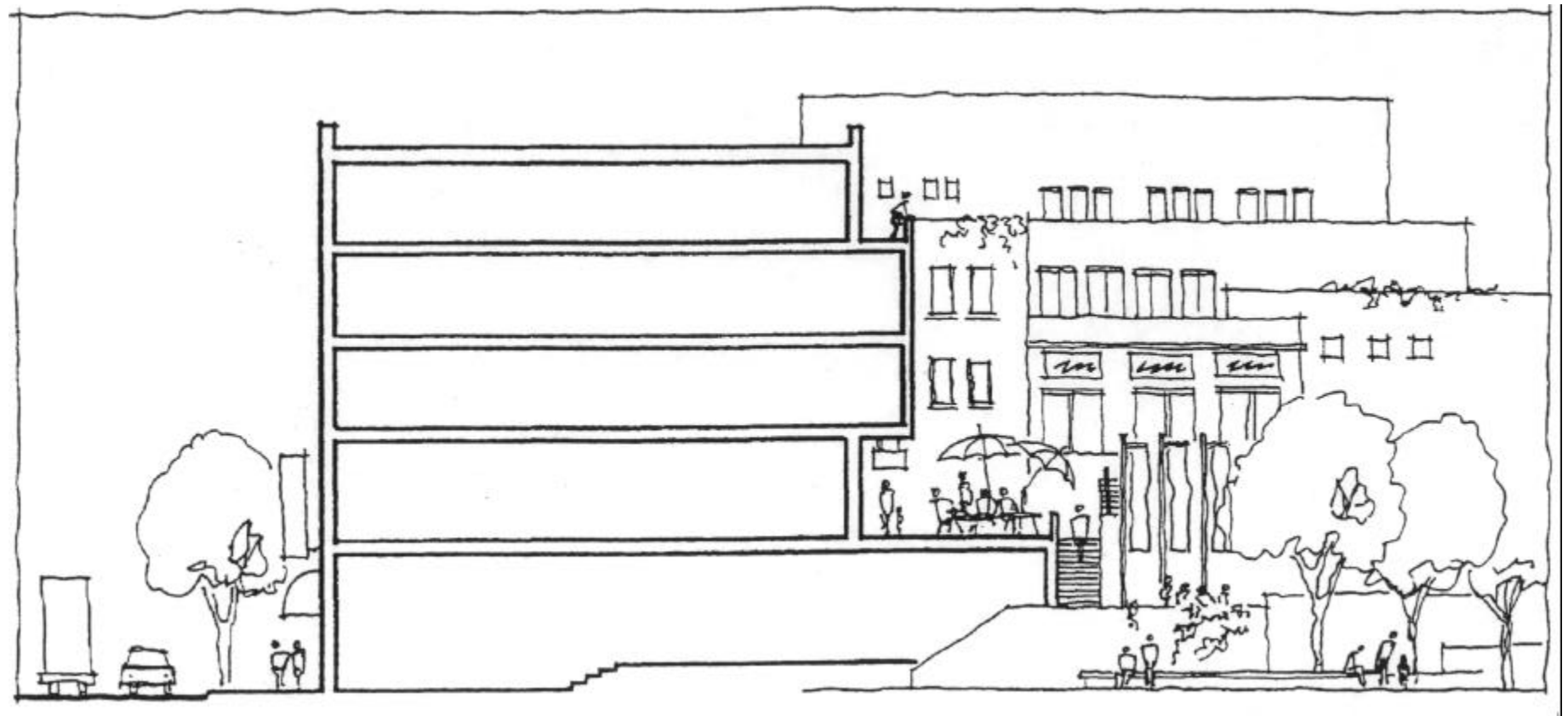
Key concepts guiding these decisions are the ideas of "permeability" and "link." The first refers to frequent visual and physical penetrations from surrounding streets into the interior of a superblock. The second emphasizes the creation of strong connections between superblocks and other parts of the Core Area, campus and surrounding community.

- ‡ At least one north-south, and one east-west connection must be provided through both blocks west of College Avenue.
- Given the limited dimension of the pseudo-superblock(s) on the east side of College, internal connections are not required.
- ‡ Paths into or through the superblocks must be continuous, although not necessarily straight. They should deform as necessary to create an intriguing pedestrian experience, or to relate to specific external influences, such as potential views into the site for motorists at the College Avenue curve, or passing through the College/Montezuma intersection.
- Special visual landmarks such as sculptures, fountains, flowering trees or unique architectural elements, should be positioned in strategic locations to enliven pedestrian walks, assist wayfinding, and mark important decision points or favorite meeting places.

Paths may be covered, for example, with a vine-covered pergola or light-weight metal or glass roof. Access through these arcades, however, must be available at all times, not just during commercial business hours.



*Lively plazas promote a memorable experience.*



*The contrast between the fixed build-to line along the street and the unregulated facades in the interior provides diversity and variety for the pedestrian.*

### Residential District

The more traditional "urban streets" approach in the Residential District allows for simpler location and massing controls, focused on creating streetscapes of appropriate types throughout the neighborhood.

#### Build-to Lines:

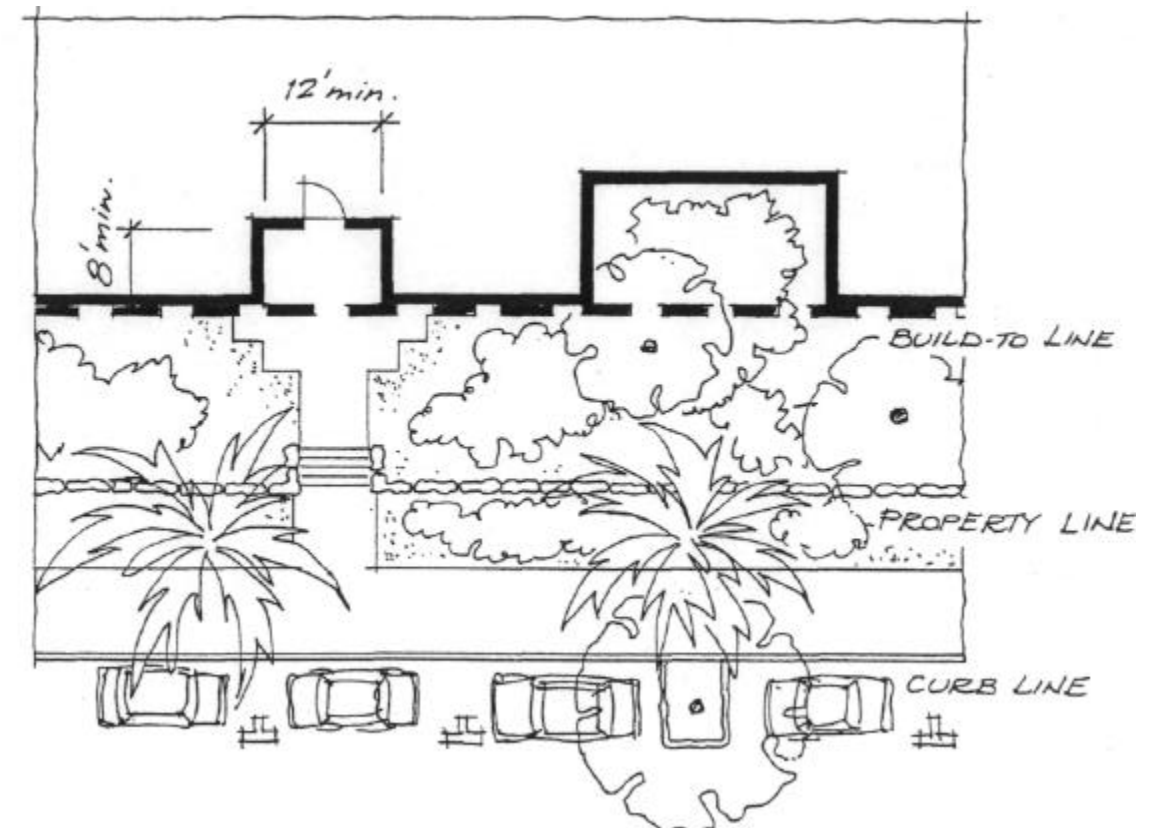
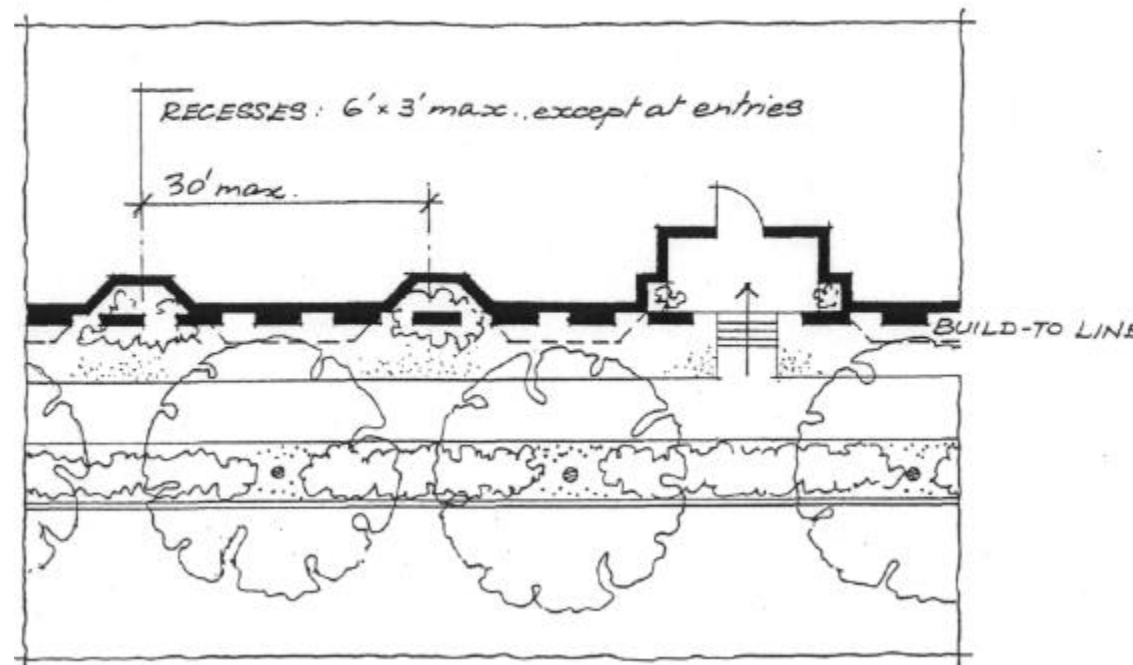
Build-to lines are established on east-west frontages throughout the area to create streets that have the feeling of well defined public spaces, or shared outdoor "living rooms."

- | On Hardy and Lindo Paseo residential frontages, finely scaled, well articulated street walls are intended. Typical dimensions for this kind of "regular-but-varied" frontage would involve approximately 75 percent to the line, 25 percent set back, in relatively small articulations that are no more than 6 feet wide by 3 feet deep. To encourage additional variety and visual interest, front entry steps of a maximum rise of 18 inches may project a maximum of 3 feet beyond the build-to line. Similarly, upper level balconies and bay windows are encouraged and may project up to 3 feet beyond the build-to line.
- This approach also applies to cooperative religious centers, but it may be negotiable based upon site arrangements of the clustered religious facilities.



*Building to the right-of-way along Campanile will create a portal into the campus.*

- | On Montezuma Road, a similarly regular but more coarsely articulated street wall is intended, providing deep, landscaped recesses in a continuous rhythm along the street, in the manner of the recently completed residential project on the north side of Montezuma. The build-to line is set 15 feet back from the property line, and only 40 percent of its length must be held by building walls. The remainder is taken up in regularly spaced, entry or garden recesses that are a minimum size of 12 feet wide by 8 feet deep. This approach also applies to the Sorority area east of College Avenue, but may be negotiable on these small sites.



**Building Setbacks:**

- | In contrast to the east-west streets, north-south frontages throughout the District maintain a building setback of 10 feet minimum. This dimension is increased to 15 feet on 55th Street to provide for the development of a densely planted landscape buffer to clearly signify the western boundary of the new neighborhood.

With a setback rather than a build-to requirement, the north-south streets will appear less formal and regulated than their east-west counterparts. Traffic will tend to move more slowly through these short blocks, and they will lend themselves to more varied and informal use, like occasional block parties and neighborhood car wash days.

- | In addition to the north-south streets, Plaza Drive also requires a setback of 10 feet from the property line. The intent is to create a frontage complimentary to the campus edge.

- It is encouraged that the Plaza Drive elevations have articulation, landscape recesses and be designed as building "fronts" rather than rear elevations. This area may be included in a solution to combine vehicular-pedestrian access along the campus boundary.

- | Minimum side yard setbacks in the Residential District are 5 feet. A 10-foot rear setback is required for properties with rear access from an alley. No rear set back, however, is required for ancillary structures such as carports, garages or garden sheds.

- Any properties abutting single family residential areas outside of the Core Sub-Area should implement sound attenuation methods, which should be designed sensitively and aesthetically to interface with the homes.

**Building Height:**

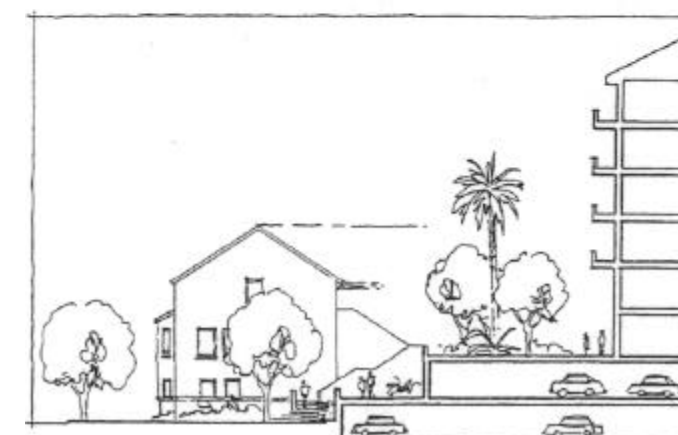
- | To further reinforce the characteristics of continuity intended for the Residential District, all frontage buildings are restricted to a maximum height of 4 stories. In blocks on the north side of Hardy, maximum heights of 6 and 12 stories are permitted, within upper level setbacks of 20 and 60 feet respectively. Between Hardy and Lindo Paseo, the interior of blocks can be built to a maximum of 6 stories, within an upper level setback of 20 feet on all sides. South of Lindo Paseo, no structure higher than 4 stories, irrespective of its location, is permitted.

- | To provide integration with the campus in the Residential District, buildings along the south side of Plaza Drive must step back an additional 20 feet from the setback line on all floors over 5 stories.

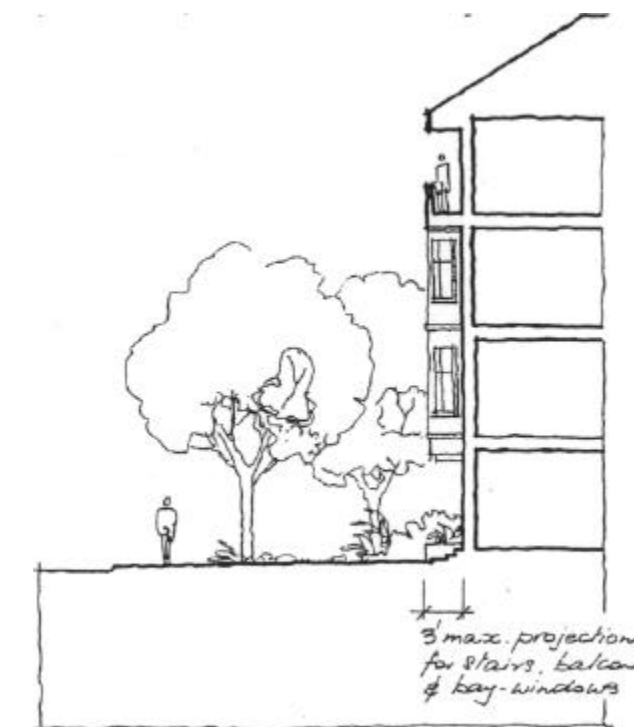
**Open Space and Pedestrian Access:**

- | Maximum lot coverage in the Residential District is stipulated at a maximum of 60 percent of gross site area. As for Mixed-Use Development, qualifying criteria for open space are not defined.

The Core Sub-Area Urban Design Plan proposes that qualifying open space in the Residential District is defined as open area, including arcades, covered walkways, public terraces and balconies, or other areas that are not building footprint, within a maximum height of 20 feet above grade. For example, semi-public open spaces constructed over 1 to 2 levels of above-grade parking qualify as required area. A percentage of the required open space may be provided off-site, so long as it is within the Residential Character District. Implementation of the recommendation and provision for open space maintenance is subject to the development of an off-site mitigation program, which may include, but not be limited to, in-lieu fees, adopted in accordance with city procedures and council policy. Active open space is encouraged over smaller, passive nodes.



Usable open space may be designed above parking garages (within 20 feet above grade).



Minimal (3 foot) projections such as balconies and bay windows may intrude into the setback.